

WARD: Cotham **CONTACT OFFICER:** Kevin Morley
SITE ADDRESS: Land To Rear Of 150 Redland Road Bristol BS6 6YD

APPLICATION NO: 15/01591/F Full Planning
EXPIRY DATE: 28 July 2015

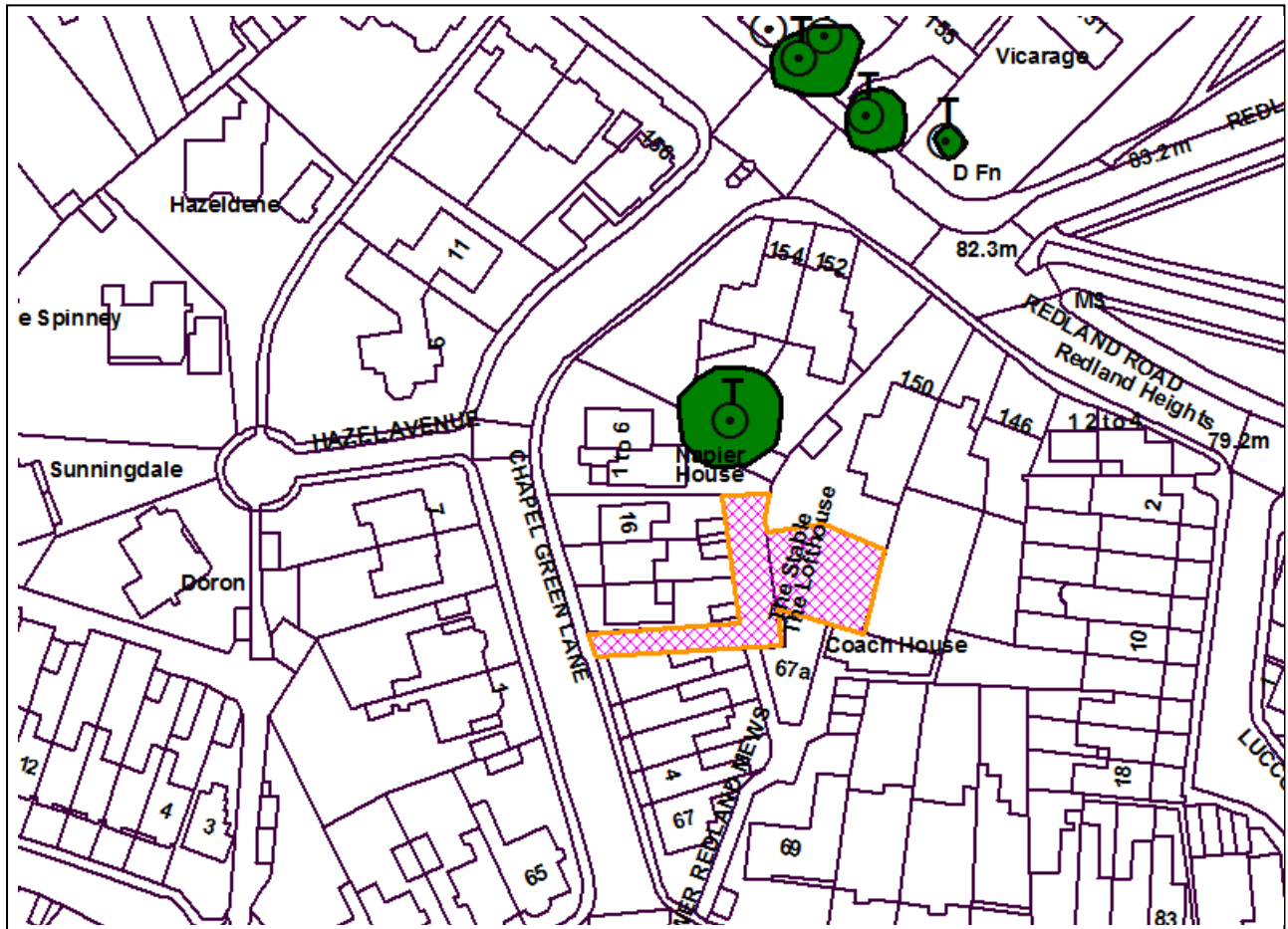
New detached two storey dwelling with new access through existing boundary wall.

RECOMMENDATION: Grant subject to Condition(s)

AGENT: Quentin Alder Architects
The Undercroft
6 Church Road
Sneyd Park
Bristol BS9 1JU
APPLICANT: Mrs M Vickers
C/o Agent

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



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SUMMARY

The application is acceptable in principle. It provides a reasonable residential environment for future occupiers whilst not causing significant harm to the amenities of neighbours. Transport and movement issues can be resolved through the imposition of conditions as can replacement planting for the lost garden trees. The proposed dwelling would become a close neighbour of properties forming a group of four backland-style houses, in a location not unduly prominent from the public realm. As such the proposal would not harm the character and appearance of the conservation area and is recommended to the committee for approval.

SITE DESCRIPTION

The application site is the southern end of the rear garden to No.150 Redland Road, a residential property arranged as 5no. flats over the basement, ground, first (x2) and second floors. The rear of the property shares a boundary with a rear tarmacked lane off Chapel Green Lane, which provides vehicular access to nearby properties including the parking area for Napier House. The site itself is private garden and supports a number of established trees. The surrounding area is characterised by residential development of various styles. The site also lies within the Cotham and Redland conservation area.

RELEVANT HISTORY

Pre-application enquiry ref.14/02878/PREAPP - for a new dwelling within the rear garden was responded to on 28th October 2014.

Planning application ref.15/01583/F for the "Demolition of an existing garage and erection of a 2 storey dwelling with basement" was WITHDRAWN on 2nd November 2015 following officer concerns relating to design and appearance within the conservation area setting. This submission was also preceded by a pre-application enquiry (ref.14/02876/PREAPP) in October 2014.

APPLICATION

The application has been amended following receipt of Transport comments, in an effort to respond to the issues raised (see below), by providing a clear layout and vehicle and pedestrian route to the proposed dwelling via the lane.

The application proposes the erection of a two-storey dwelling house with three bedrooms. The property would have a hipped roof and a combination of red-brick and timber cladding finish. The application includes off-street parking, refuse/recycling storage, cycle parking, private amenity space and the laying out of a pedestrian route along the lane.

RESPONSE TO PUBLICITY AND CONSULTATION

The application was advertised via press and site notices. Consultation letters were also sent to 55no. nearby occupiers. As a result 35no. representations were received including 34no. objections and a statement of support from the Redland and Cotham Amenities Society. Objections raised are summarised as follows: -

- Over-development of the site
- Heritage conservation

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- Development is out of character
- Design/appearance
- Residential amenity including noise and disturbance from cars
- Traffic and transport including highway safety
- Construction traffic
- Refuse/recycling
- Loss of established trees/green infrastructure/habitat
- Sustainability/climate change
- Drainage

The above topics will feature within the Key Issue considerations of the report. Other issues raised not directly relating to planning considerations related to: -

- Private access rights along the lane [it is noted that the Applicant has illustrated that they have certain ownership rights over the lane and planning permission would not overrule any legal rights on the land in any event].
- Affordable housing [in this instance the quantum of development means that the application does not trigger the council's planning policy in relation to the provision of affordable housing].
- Sewers and current capacity [this is not a material consideration in this instance].
- Impact on property prices [this is not a material consideration].
- Smaller proposal/bungalow would be more appropriate [the application is to be assessed on the basis of what has been applied for].

OTHER COMMENTS

BCC TRANSPORT DEVELOPMENT MANAGEMENT commented as follows: -

Transport Development Management recommends refusal of this application as it is contrary to policy BCS10 of the adopted Core Strategy and policy DM23 of the Site Allocations and Development Management document. Unfortunately neither of these policies is considered in the Design and Access Statement.

The matters for assessment in relation to highways matters are fourfold:

- 1) whether safe access can be achieved to the development for non-motorised users associated with it;
- 2) how much additional vehicular movement will be generated into and out of the new dwelling and whether or not this generates a safety concern;
- 3) whether access to the property is acceptable for emergency vehicles and servicing, and
- 4) the storage and collection of refuse and recycling.

These matters are considered below in reverse order.

In terms of the waste collection and in relation to BCC guidance, residents should not be expected to transport bins / recycling to a collection point over a length greater than 30m. The proposed refuse store is located 40m from the nearest adopted highway (the footway on Chapel Green Lane). Notwithstanding the distance, this arrangement generates a further concern in that it is difficult to see how refuse could be stored on collection days in such a way as not to cause blockage and obstruction to the access lane or indeed the footway of Chapel Green Lane itself, which is used by a considerable number of schoolchildren at peak times.

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With regard to emergency access and as confirmed by Fire officers in Manual for Streets, the maximum distance from the highway a dwelling is able to be attended by a fire appliance is 45 metres. The proposed dwelling is in excess of 50 metres and therefore fails to meet these criteria. Further concerns result when deliveries and removals are taken into account and the inability of the lane to accommodate these movements.

Of less concern are the levels of additional traffic that the proposal will generate. The traffic generation generated by the additional dwelling is not considered to be significant.

However, on the basis that the lane would be the sole access to the proposed property for pedestrians and cyclists, and taking into account its current dimensions, this would present problems in the event that new and existing vehicles were to encounter a pedestrian, or in particular a pushchair or mobility scooter along the lane, who would have nowhere to go in order to avoid a motor vehicle. This problem becomes more apparent in view of the size of the dwelling and the likelihood of it providing family accommodation.

BCC URBAN DESIGN AND CONSERVATION commented as follows: -

Refer to policy and guidance.

RELEVANT POLICIES**National Planning Policy Framework – March 2012****Bristol Core Strategy (Adopted June 2011)**

BCS5	Housing Provision
BCS9	Green Infrastructure
BCS13	Climate Change
BCS14	Sustainable Energy
BCS15	Sustainable Design and Construction
BCS16	Flood Risk and Water Management
BCS18	Housing Type
BCS20	Effective and Efficient Use of Land
BCS21	Quality Urban Design
BCS22	Conservation and the Historic Environment
BCS23	Pollution

Bristol Site Allocations and Development Management Policies (Adopted July 2014)

DM1	Presumption in favour of sustainable development
DM17	Development involving existing green infrastructure
FDM21	Development of private gardens
DM23	Transport development management
DM26	Local character and distinctiveness
DM27	Layout and form
DM28	Public realm
DM29	Design of new buildings
DM31	Heritage assets
DM32	Recycling and refuse provision in new development

Supplementary Planning Guidance

Cotham and Redland Conservation Area Character Appraisal

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KEY ISSUES

(A) ARE THE PROPOSALS ACCEPTABLE IN PRINCIPLE?

The key policy consideration in this case is policy DM21: Development of Private Gardens. This policy is designed to assess the principle of such infill developments and states that development "...involving the loss of gardens will not be permitted unless... (i) The proposal would represent a more efficient use of land at a location where higher densities are appropriate; or (ii) The development would result in a significant improvement to the urban design of an area."

The proposed development only needs to satisfy one of the above criterion, but in all cases, any development of garden land should not result in harm to the character and appearance of an area.

The site is located approximately 370m to nearest designated centre, in this case Whiteladies Road town centre. The distance from the centre is critical in ascertaining whether a site is suitable for increased housing densities, or in this case infill housing, as new development should be accessible to local services including frequent public transport routes, as described in policy BCS20 of the Core Strategy. The site is considered to be in an accessible location in these respects, less than 400m (or a five minute walk) from the centre, therefore satisfying part (i) of policy DM21.

Therefore, subject to assessing the harm to the character and appearance of the area (Key Issue C below), and other considerations in this report; the proposal is considered acceptable in principle.

(B) DO THE PROPOSALS SAFEGUARD THE RESIDENTIAL AMENITY OF NEIGHBOURING AND FUTURE OCCUPIERS OF THE SITE?

Future occupiers:

The proposed dwelling provides an acceptable residential environment for future occupiers of the site, offering a reasonable outlook and sufficient internal and external space to function as a family-sized three bedroom house.

Privacy/overlooking impacts:

The layout of the proposed dwelling supports principle windows to the west elevation, maintaining a suitable 25m privacy distance from neighbours. Ground floor windows within the end elevations do not give rise to privacy concerns given the intervening boundaries. It is considered that subject to ensuring the bathroom window is obscurely glazed and that no further windows can be inserted beyond those on the plans; it is considered that the proposal would not result in any unacceptable loss of privacy.

Overshadowing/overbearing impacts:

The supplied shadow analysis within the Design and Access Statement illustrates that the proposed two-storey dwelling would have limited impact in terms of overshadowing of neighbouring gardens. Notwithstanding, it is considered that what impact that would occur, would be concentrated on the southern end of the neighbouring garden (No.148 Redland Road); away from the property and the principle windows within their rear elevation. It is also considered that no significant harm would occur to the occupiers living within the three other backland style houses, to the south of the site (The Lofthouse, The Stable and Coach House).

The presence of a two-storey building in the proposed location will create some additional overbearing impact beyond the existing situation. This would be most apparent to the rear conservatory of the basement flat of No.150 itself, some 11m away from the proposal's flank wall. However, given the change in levels, this impact would diminish to some degree and given the glazed nature of the conservatory; no objections are raised.

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It is considered that given the topography of the site and the location of the proposed house; the development would not result in any significant harm in terms of overshadowing and/or overbearing impact, subject to ensuring future permitted development rights in relation to extensions are restricted.

Noise and disturbance:

It is accepted that introducing a new residential unit into an area that is currently garden land will result in additional noise and disturbance to neighbours, from the general comings and goings of future occupiers, but this activity would be residential in nature, so no objection is raised. In this instance the proposals also include the provision of off-street vehicle parking, which could further add to noise and disturbance from its usage.

The lane that the site is accessed from provides vehicle access to a number of properties, including a large car-port for use by Napier House residents (at least 6no. vehicles). The introduction of a new dwelling's worth of activity would not significantly alter the existing situation to warrant objection and the proposed house itself would act as a solid barrier to reduce noise and light spill (from car headlights) affecting the gardens and properties to the east/north-east. The proposal would not result in any unacceptable harm from noise and disturbance.

It is considered that overall; the proposals provide for the residential amenities of future occupiers and would not unacceptably harm those of existing neighbours.

(C) ARE THE PROPOSALS VISUALLY ACCEPTABLE AND DO THEY PRESERVE AND/OR ENHANCE THE CHARACTER AND APPEARANCE OF THE CONSERVATION AREA?

Subject to full large-scale design details and further information on the intended finishes to the proposed dwelling; within the context of this backland location adjacent to similar infill-style houses; the proposals are considered to be visually acceptable.

The site is located within the Cotham and Redland Conservation area. The site is located centrally within an irregularly shaped perimeter block formed by properties on Redland Road, Luccombe Hill, Lower Redland Road and Chapel Green Lane. At the centre of this perimeter block there are three other back-land style houses, off a lane from Lower Redland Road (The Lofthouse, The Stable and Coach House). The proposal would be similar in form and design as these properties but would also benefit from private amenity space. It is also noted that the location of the proposed dwelling is mostly obscured from public viewing points, even when attempts are made to glimpse the site from Chapel Green Lane or higher ground near Napier House.

It is accepted that the proposals would erode the size of the garden of No.150 Redland Road, which currently serves the basement flat; but this garden is one of the largest gardens in the immediate area and its reduction in size is not considered harmful to the character or appearance of the conservation area. Furthermore, given the immediate context in which the proposed house will be measured; the introduction of an additional dwelling within this location is considered to be acceptable subject to details, as described above.

As such, given the discrete location of the site and the pattern of development that has evolved in this backland location; it is considered that the proposals would preserve the character and appearance of the conservation area.

(D) DO THE PROPOSALS RAISE ANY TRANSPORT AND MOVEMENT ISSUES?

The site is located in a sustainable location, close to local services within the Whiteladies Road town centre, to the west. The proposals include suitable cycle parking, which would be conditional of any permission.

Concerns have been raised by the Transport Development Management Team regarding the safety of

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the access lane and its use as a shared surface by vehicles and pedestrians. The Agent has provided additional clarity on the proposed site plans that shows how the lane can be delineated to allow for a pedestrian walkway whilst maintaining sufficient width to allow cars to pass. This appears satisfactory and further details can be provided through the submission of a wider-landscaping plan to show how this private access will be treated to serve the development and the existing dwelling to the south, which also use the lane for vehicular access.

Another concern raised was the distance for emergency services accessing the site, especially a fire rescue appliance, to reach the proposed house. The proposal is within the 50m limit stated by Transport DM, but not all parts of the proposed house are. This said an advisory note would be attached to any permission noting that advice should be sought by the Applicant from the Local Fire Authority as to whether they need to include a sprinkler system within the property to satisfy the relevant element of the building code/practice.

The final concern raised by the Team was related to refuse/recycling and the distance to traverse in taking bins to the highway and returning them for collection purposes. It was of particular note that the distances involved may result in receptacles being left on the highway (Chapel Green Lane); posing an obstruction. In response to this the Agent has revised the scheme to move the on-site bin store closer to the edge of the site and thus closer to the highway - a direct distance of approximately 30m. This is considered to be the maximum allowable distance and given the family style housing proposed, it would seem reasonable that bins would be brought back in after collection days. In any event a condition stating that bins cannot be left for prolonged periods on the highway, would resolve this issue.

Objection has been also raised by neighbouring occupiers with regards to construction access and the use of the lane and any infringement upon it during the build. Given the narrowness of the lane and the amount of usage it supports, any permission on the site would be subject to a pre-commencement planning condition requiring the contractor to provide the council with a construction management plan; so that the inevitable interruptions/inconveniences caused by building the house would be kept to a minimum and that highway safety is upheld throughout.

Subject to the above conditions and the provision of the pedestrian and vehicle access points in accordance with the plans; it is considered that the proposals do not raise any unacceptable transport and movement issues.

(E) DO THE PROPSALS RAISE ANY TREE AND/OR NATURE CONSERVATION ISSUES?

The proposals would result in the loss of 9no. established trees within the site and would retain 3no. The trees to be lost have been categorised as Class C within the arboricultural report, individually offering limited amenity to the area. However, the loss of these trees would require mitigation in accordance with the Bristol Tree Replacement Standard, as described within policy DM17: Development Involving Existing Green Infrastructure. As 5no. of the trees to be lost have a comparatively narrow trunk diameter, the replacement tree planting requirement is a range of between 5-10no. trees, which could be accommodated within the landscaping of the development. Subject to the provision of suitable replacement planting via a landscaping planning condition; it is considered that the proposals do not raise any unacceptable issues relating to trees.

Furthermore, as part of the landscaping scheme, further planting of fruit bearing trees and bushes, of local provenance, would provide suitable replacement habitat for the trees (some of which are fruit trees) to be lost.

(F) DO THE PROPOSALS MEET THE REQUIREMENTS OF SUSTAINABLE DEVELOPMENT?

The application is supported by a Sustainability Statement demonstrating compliance with the development plan policies relevant to this issue. Subject to the adherence to the recommendations of

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the report, the final details of the renewable energy equipment and the inclusion of permeable surfaces within the landscaping scheme; the proposals do not raise any unacceptable issues relating to sustainable development policies.

CONCLUSION

The application has been assessed carefully against policy DM21 and due weight has been given to amenity considerations and the potential impact the dwelling would have on the character and appearance of the area. A pragmatic approach has been taken to transport and movement issues and in conclusion, it is considered that with the application of planning conditions the proposals are acceptable and as such are recommended to the committee for approval.

COMMUNITY INFRASTRUCTURE LEVY

How much Community Infrastructure Levy (CIL) will this development be required to pay?

The CIL liability for this development is £6,667.50.

RECOMMENDED GRANT subject to condition(s)

Time limit for commencement of development

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

Pre commencement condition(s)

2. Construction management plan

No development shall take place including any works of demolition until a construction management plan or construction method statement has been submitted to and been approved in writing by the Local Planning Authority. The approved plan/statement shall be adhered to throughout the construction period. The statement shall provide for:

- a) Parking of vehicle of site operatives and visitors,
- b) routes for construction traffic,
- c) hours of operation,
- d) method of prevention of mud being carried onto highway,
- e) pedestrian and cyclist protection,
- f) proposed temporary traffic restrictions, and
- g) arrangements for turning vehicles.

Reason: In the interests of safe operation of the highway in the lead into development both during the demolition and construction phase of the development.

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3. Landscaping - Further Details

Notwithstanding the information submitted to date, no building or use hereby permitted shall be occupied or the use commenced until there has been submitted to and approved in writing by the Local Planning Authority a scheme of hard and soft landscaping, which shall include:

- a) Proposed boundary treatments, including heights, materials and appearance,
- b) Proposed hard-standing material(s) and details of permeability,
- c) Proposed layout markings for the access lane, and
- d) Proposed planting schedule including replacement trees in accordance with the Bristol Tree Replacement Standard.

The approved scheme shall be implemented so that planting can be carried out no later than the first planting season following the completion of the development. All planted materials shall be maintained for five years and any trees or plants removed, dying, being damaged or becoming diseased within that period shall be replaced in the next planting season with others of similar size and species to those originally required to be planted unless the council gives written consent to any variation.

Reason: To preserve the character and appearance of the site and conservation area, to mitigate the loss of existing trees, to reduce surface water run-off and to ensure appropriate front boundary treatment.

4. Protection of Retained Trees During the Construction Period

No work of any kind shall take place on the site until the protective fences have been erected around the 3no. retained trees in the position and to the specification shown within the arboricultural report, prepared by Hillside Trees Ltd. The Local Planning Authority shall be given not less than two weeks prior written notice by the developer of the commencement of works on the site in order that the council may verify in writing that the approved tree protection measures are in place when the work commences. The approved fences shall be in place before any equipment, machinery or materials are brought on to the site for the purposes of the development and shall be maintained until all equipment, machinery and surplus materials have been removed from the site. Within the fenced areas there shall be no scaffolding, no stockpiling of any materials or soil, no machinery or other equipment parked or operated, no traffic over the root system, no changes to the soil level, no excavation of trenches, no site huts, no fires lit, no dumping of toxic chemicals and no retained trees shall be used for winching purposes. If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the council.

Reason: To protect the retained trees from damage during construction, including all ground works and works that may be required by other conditions, and in recognition of the contribution which the retained trees give and will continue to give to the amenity of the area.

5. Further details of before relevant element started

Notwithstanding the information submitted to date, detailed drawings and/or manufacturer's details of the following shall be submitted to and be approved in writing by the Local Planning Authority before the relevant part of work is begun. The detail thereby approved shall be carried out in accordance with that approval.

- a) Typical details of facing brick, timber boarding and roof tiles,
- b) Typical details of eaves, soffits, overhangs, and ridge,

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- c) Typical details of all types of window openings including sills, reveals and design of external doors and windows,
- d) Typical details of all balcony railings,
- e) Typical details of the west elevation canopy,
- f) Typical details of rain water goods.

Reason: To ensure the appearance of the building is satisfactory and in to preserve the character and appearance of the conservation area.

Pre occupation condition(s)

6. Renewable Energy Equipment

Prior to the first occupation of the dwelling hereby permitted, further details including the final roof layout, visual appearance, method of fixing, and technical specifications (including the output) of the renewable energy equipment shall be submitted to and approved in writing by the Local Planning Authority. The equipment shall be installed in accordance with the approved details and made fully operational prior to the first occupation of the dwelling and maintained in situ in accordance with the approved details and as fully operational at all times thereafter.

Reason: To ensure that the development would meet sustainability and climate change policy objectives.

7. Sustainability Statement

The development hereby approved shall be carried out in accordance with the measures contained within the approved Sustainability Statement, prepared by Environomic and dated 14th March 2015, prior to the first occupation of the dwelling hereby approved.

Reason: To ensure that sustainability policy objectives would be met.

8. Completion of Pedestrians/Cyclists Access - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of access for pedestrians and/or cyclists have been constructed/laid out in accordance with the approved plans, refs.1418.022A & 1418.023A, and shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

9. Completion of Vehicular Access - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of vehicular access has been constructed and completed in accordance with the approved plans, refs.1418.022A & 1418.023A, and the said means of vehicular access shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

10. Implementation/Installation of Refuse Storage and Recycling Facilities - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the refuse store, and area/facilities allocated for storing of recyclable materials, as shown on the

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approved plans have been completed in accordance with the approved plans. Thereafter, all refuse and recyclable materials associated with the development shall either be stored within this dedicated store/area, as shown on the approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the public highway or pavement, except on the day of collection.

Reason: To safeguard the amenity of the occupiers of adjoining premises, protect the general environment, and prevent obstruction to pedestrian movement, and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

Post occupation management

11. No further extensions

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and/or re-enacting that Order) no extension or enlargement (including additions to roofs) shall be made to the dwellinghouse hereby permitted, or any detached building erected, without the express permission in writing of the council.

Reason: The further extension of this dwelling or erection of detached building requires detailed consideration to safeguard the amenities of the surrounding area.

12. No Further Windows

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and/or re-enacting that Order) no windows, including rooflights, other than those shown on the approved plans shall at any time be placed in the dwellinghouse hereby permitted without the grant of a separate planning permission from the Local Planning Authority.

Reason: To safeguard the amenities of the adjoining premises from overlooking and loss of privacy.

13. Non opening and obscured glazed window

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and/or re-enacting that Order) the window within the south flank elevation at first-floor level shall be non-opening and glazed with obscure glass to a specification to be agreed with the Local Planning Authority and shall be permanently maintained thereafter as non-opening and obscure glazed.

Reason: To safeguard the amenities of the adjoining premises from overlooking and loss of privacy.

List of approved plans

14. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

Location plan, received 27 March 2015

1418.001 Existing block plan, received 27 March 2015

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1418.002 Existing site plan, received 27 March 2015
1418.003 Existing garden elevations, received 27 March 2015
1418.024 Proposed elevations, received 27 March 2015
1418.22A Proposed site and roof plans, received 25 September 2015
1418.23A Proposed plans and section, received 25 September 2015
Arboricultural report, received 27 March 2015
Sustainability statement, received 27 March 2015
Design and access statement, received 27 March 2015

Reason: For the avoidance of doubt.

Advices

1. The Applicant is advised to seek further advice and recommendations from the Technical Fire Safety Officer of Avon Fire and Rescue when devising detailed construction and Building Regulation plans (in relation to the need for fire prevention and sprinkler system).

BACKGROUND PAPERS

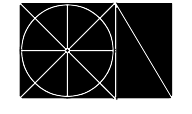
Transport Development Management
Urban design

1 July 2015
1 July 2015



Rev.	Date	Notes	Drawn

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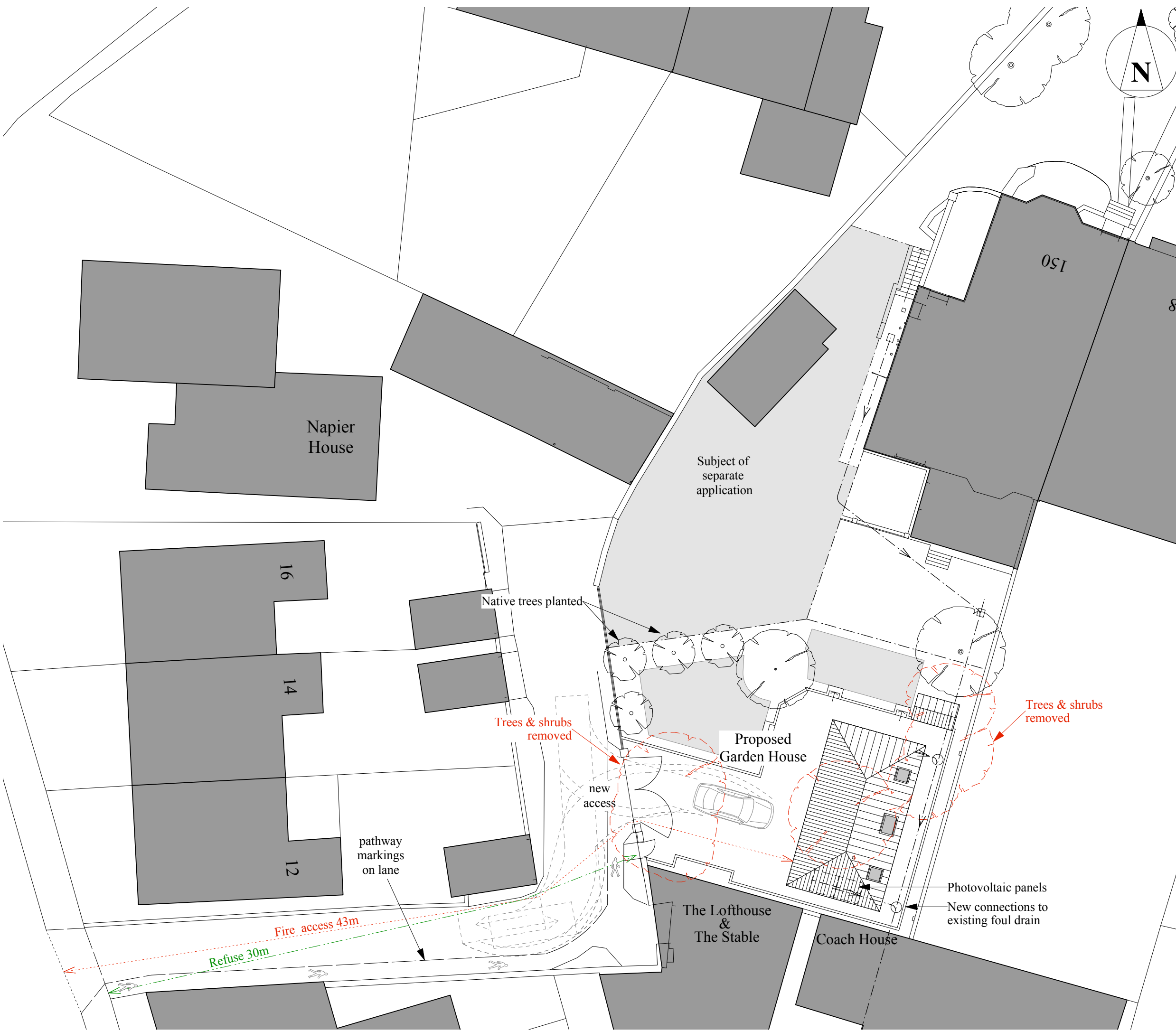
QUENTIN ALDER
 ARCHITECTS

The Undercroft, 6 Church Road,
 Sneyd Park, Bristol BS9 1JU
 Telephone: 0117 968 3111 Facsimile: 0117 968 7333
 E-mail: mail@QuentinAlder.co.uk

150 Redland Road

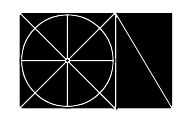
Block Plan
 Existing

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Rev.	Date	Notes	Drawn
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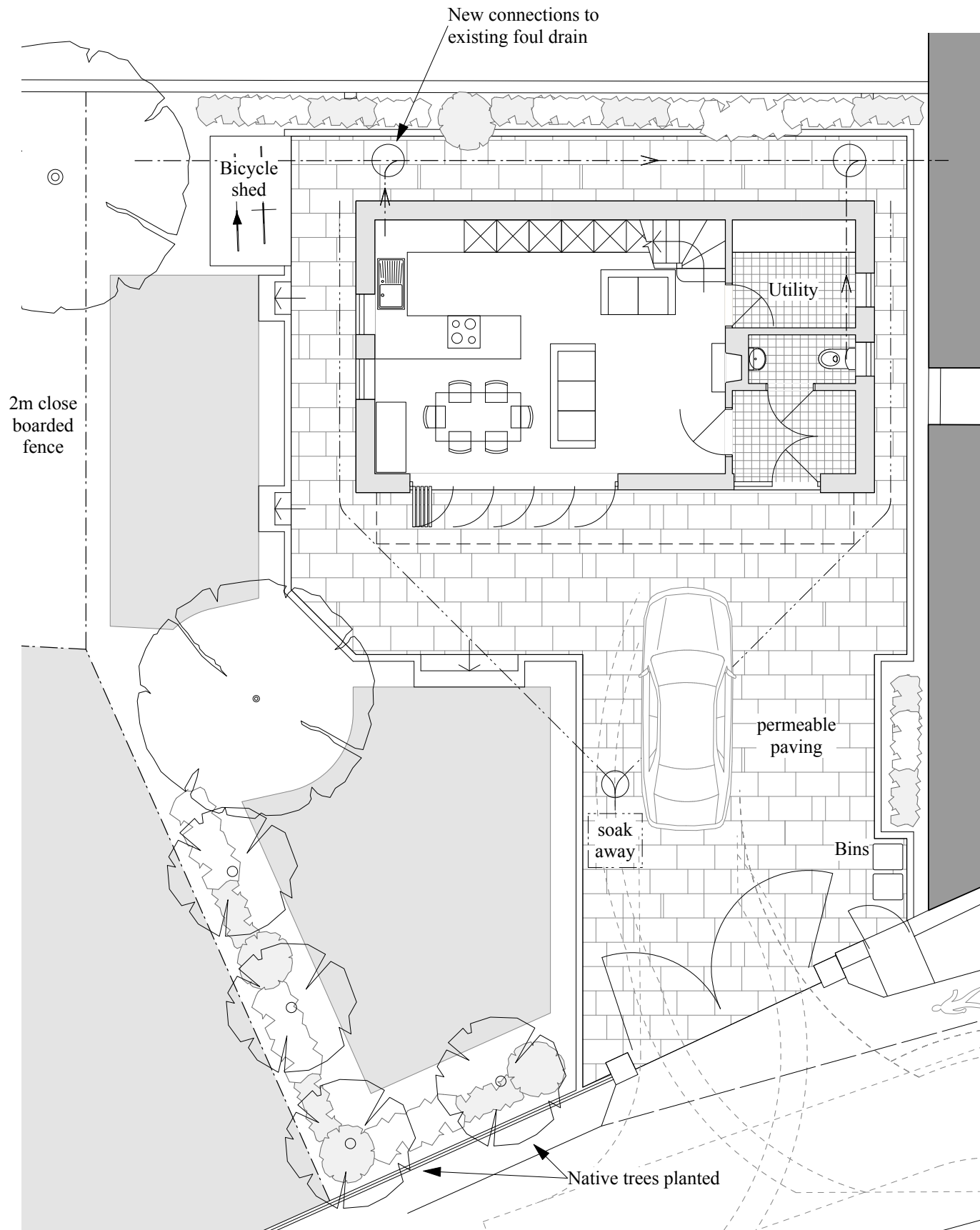
Telephone: 0117 968 3111 Facsimile: 0117 968 7333
 E-mail: mail@QuentinAlder.co.uk

Garden House
 150 Redland Road

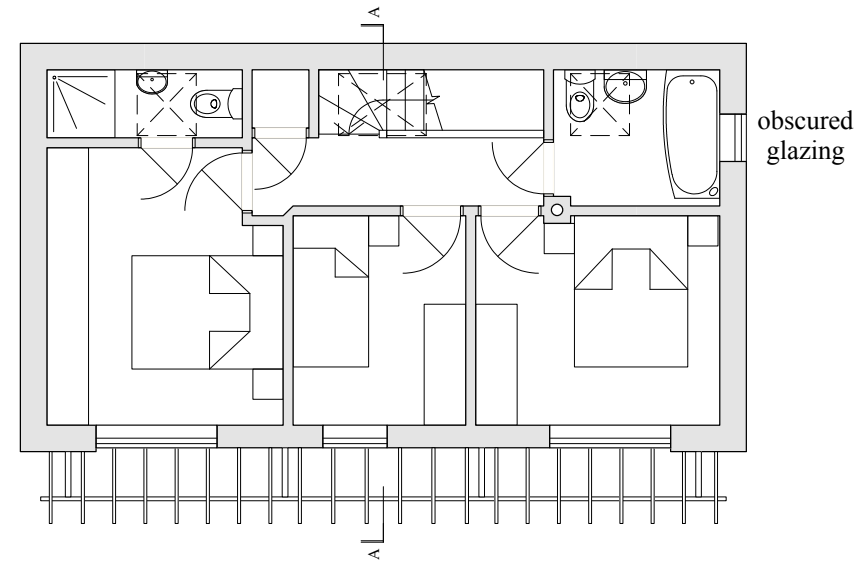
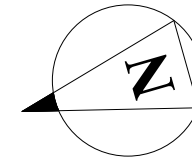
for
 Mrs M Vickers

Site and Roof Plan
 Proposed

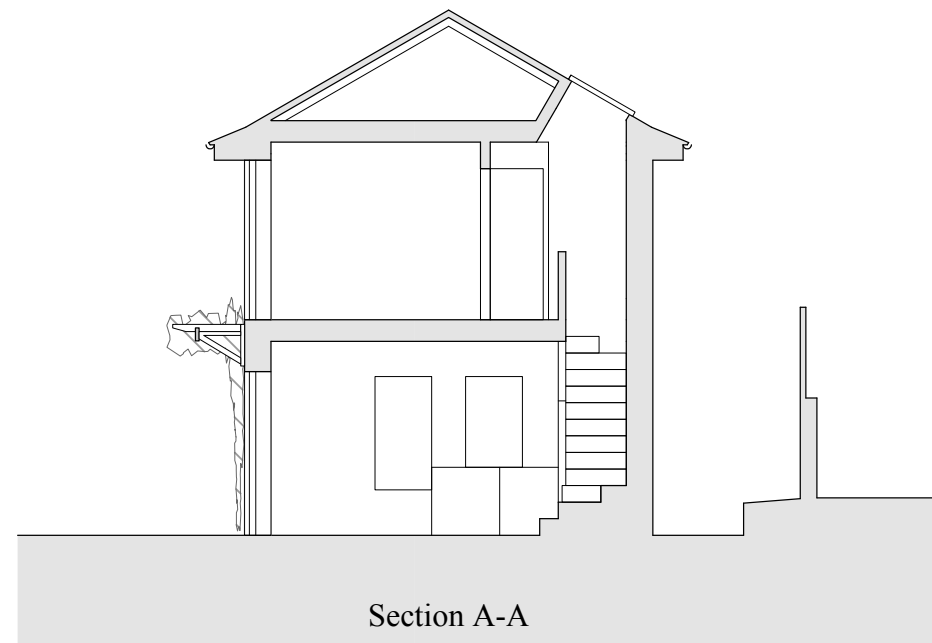
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Ground Floor
And Garden Plan

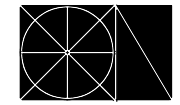


First Floor Plan



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QUENTIN ALDER
ARCHITECTS

The Undercroft, 6 Church Road,
Sneyd Park, Bristol BS9 1JU

Telephone: 0117 968 3111 Facsimile: 0117 968 7333
 E-mail: mail@QuentinAlder.co.uk

Garden House
150 Redland Road

for
Mrs M Vickers

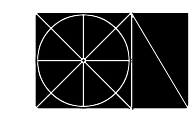
Floor Plans and Section
Proposed

Scale	1:100 @A3	Drawn	JAR
Drawing No.	1418.023A	Date	19.03.15

Materials:
 Red brickwork with vertical timber boarding to front.
 Brick head & sill details to side elevations.
 Red clay double roman roof tiles.
 Painted timber doors, windows and trellis.

Rev.	Date	Notes	Drawn

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 Figured dimensions take precedence. Check all dimensions on site
 Report any discrepancy to Architect



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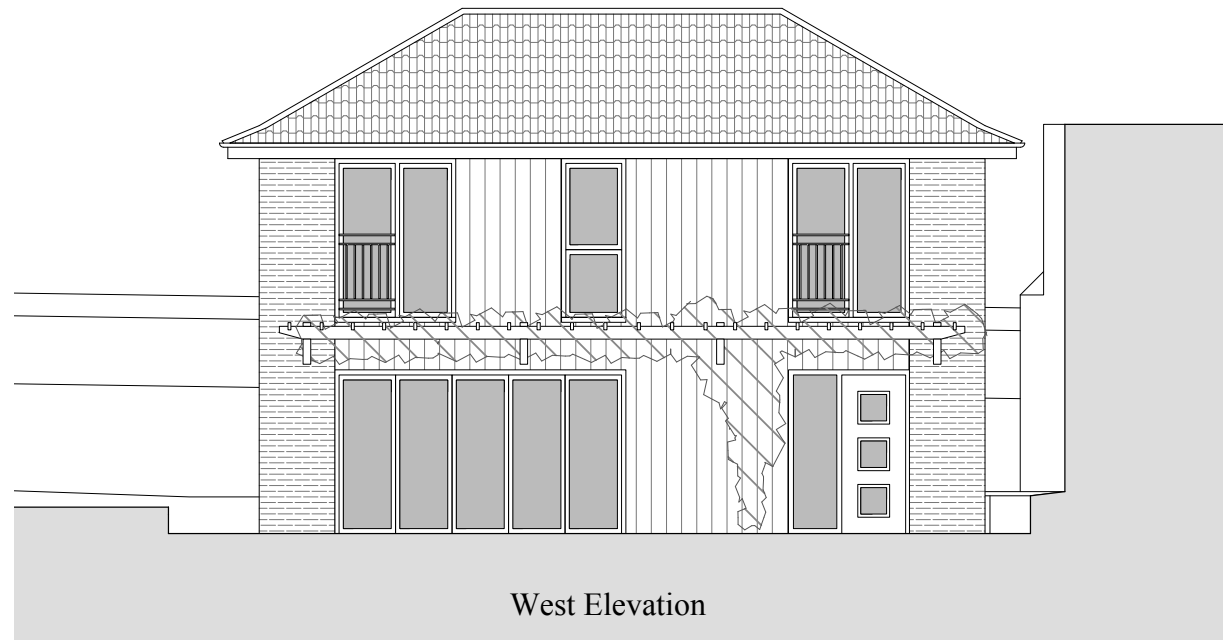
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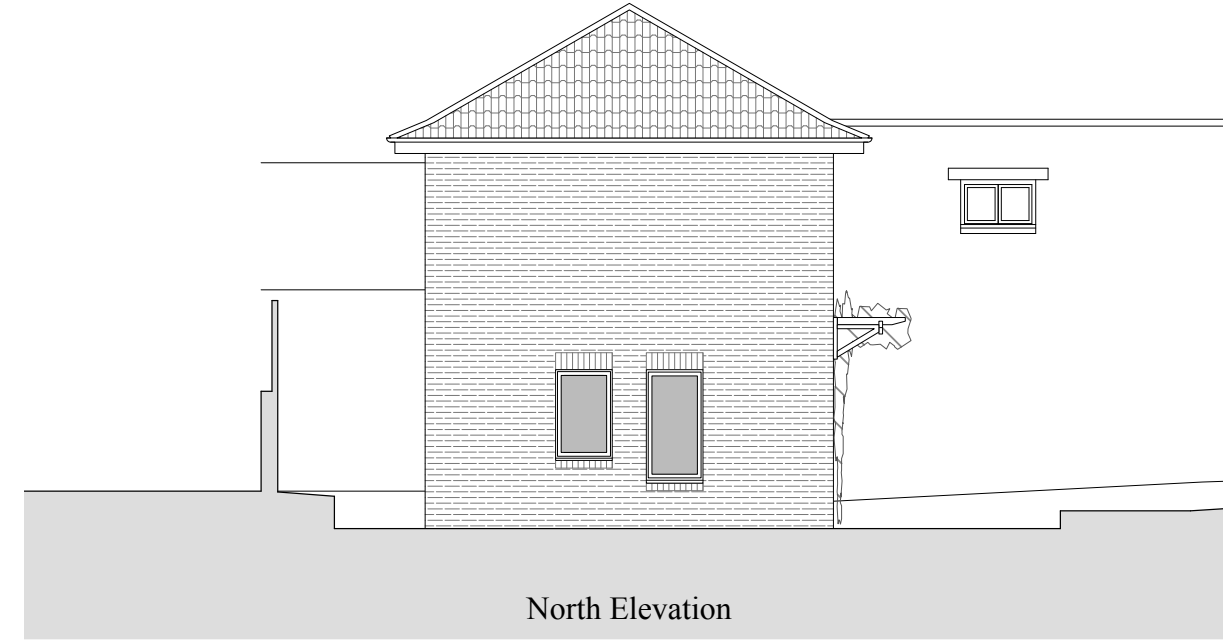
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Elevations
 Proposed

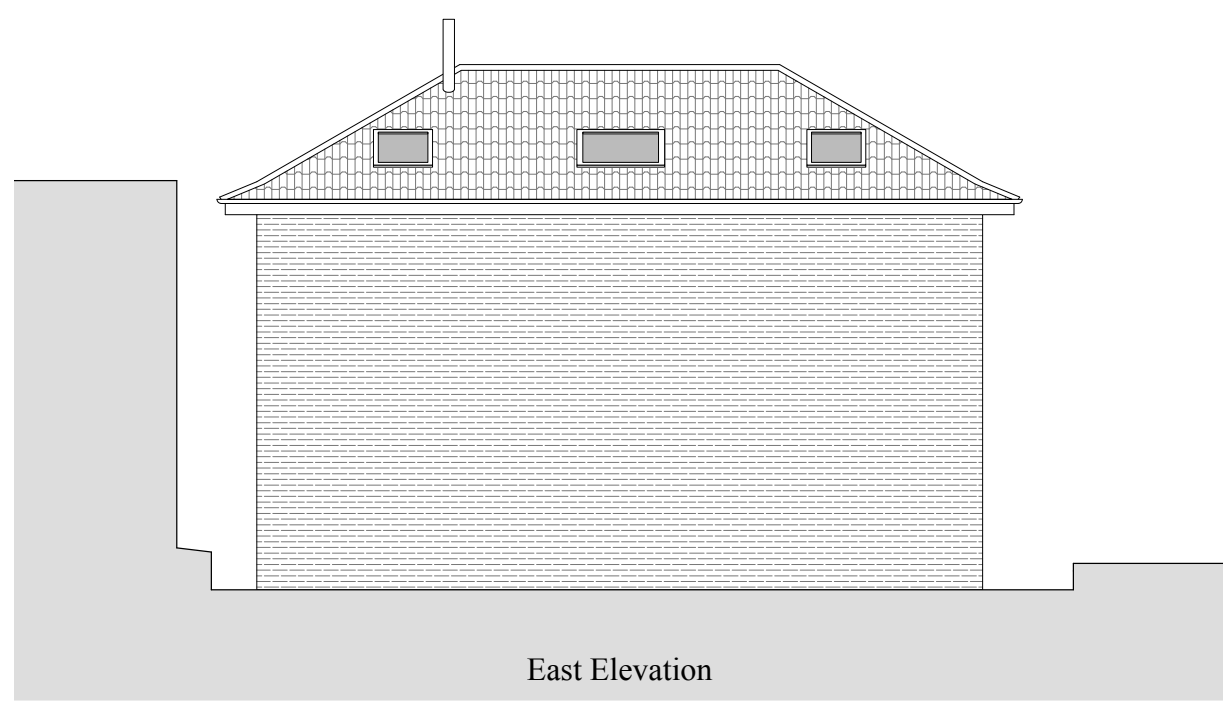
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Drawing No.	1418.024	Date	19.03.15



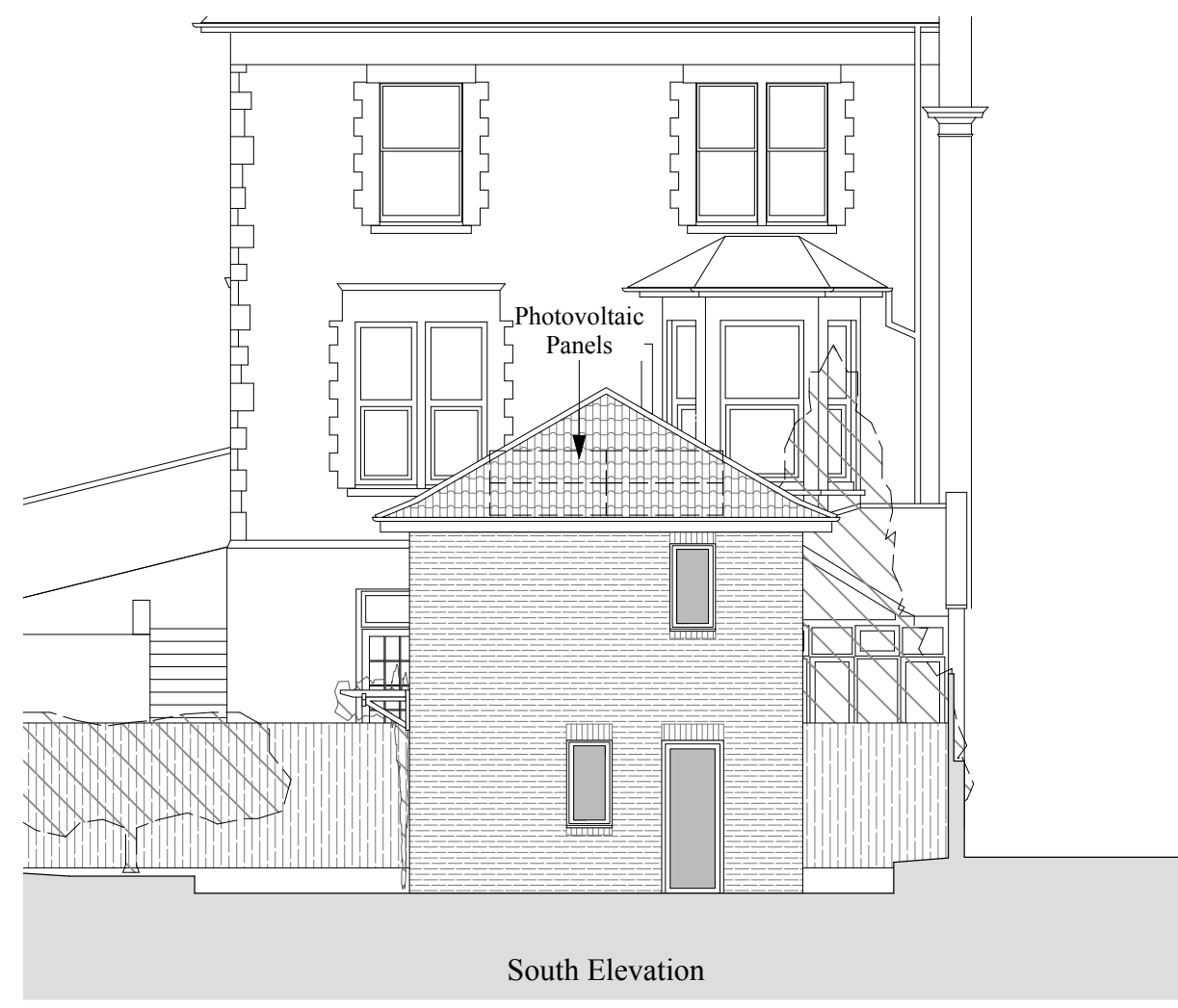
West Elevation



North Elevation



East Elevation



South Elevation